

Park Street Car Park

Planning Guidance Note



CAMBRIDGE
CITY COUNCIL

March 2016

■ Front cover picture by Matthew Paul

This publication has been produced by:

Cambridge City Council
Urban Design & Conservation Team
PO Box 700
Cambridge
CB1 0JH

Tel: 01223 457000

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1. INTRODUCTION AND BACKGROUND

1.1. Introduction and Background

1.1.1. Cambridge City Council own and manage Park Street Car Park. The proposal to demolish the existing structure and redevelop the site to provide a replacement car park, cycle park, residential and commercial uses have been agreed by the Council's Strategy and Resources Committee. External consultants have advised on the viability of the redevelopment of the site which has created the following requirements as summarised in the report 'Future of Park Street Car Park' taken to Strategy and Resources Committee on the 18th January 2016. This follows on from the Strategy and Resources Committee (12th October 2015) where the Leader of the Council approved the following requirements for the redevelopment of Park Street Car Park:

- 250 space underground car park
- Above ground, a mixed development of market, intermediate and social housing, including an option for commercial rental on the ground floor, in conjunction with cycle parking
- The same or greater number of cycle parking spaces

1.2. Purpose and Scope of the Brief

1.2.1. The purpose of this planning guidance note is to summarise clearly and concisely the planning context and constraints as well as to establish key development principles for the redevelopment of the Park Street Car Park site. This guidance will help to ensure that the best and most efficient use of the land is delivered and will help with the future disposal of the

site through providing clear information on the form of development likely to be acceptable. It will assist Officers tasked with making recommendations on future redevelopment proposals and Members tasked with making decisions on planning applications.

1.2.2. Whilst this guidance provides a thorough understanding of the key planning matters relating to this site, it cannot account for subsequent changes in policy at a national or local level or in guidance and standards that may occur beyond the point of publication and approval. It should also be noted that this brief does not repeat existing guidance or policies but instead will make reference to other documents and advice where relevant. Future development proposals will need to be consistent with approved national and local policies in place at the time of the determination of planning applications. It will be for the site promoter to retain suitably qualified expertise to advise them of all planning related matters.



Figure 1: View of Park Street Car Park looking north-west

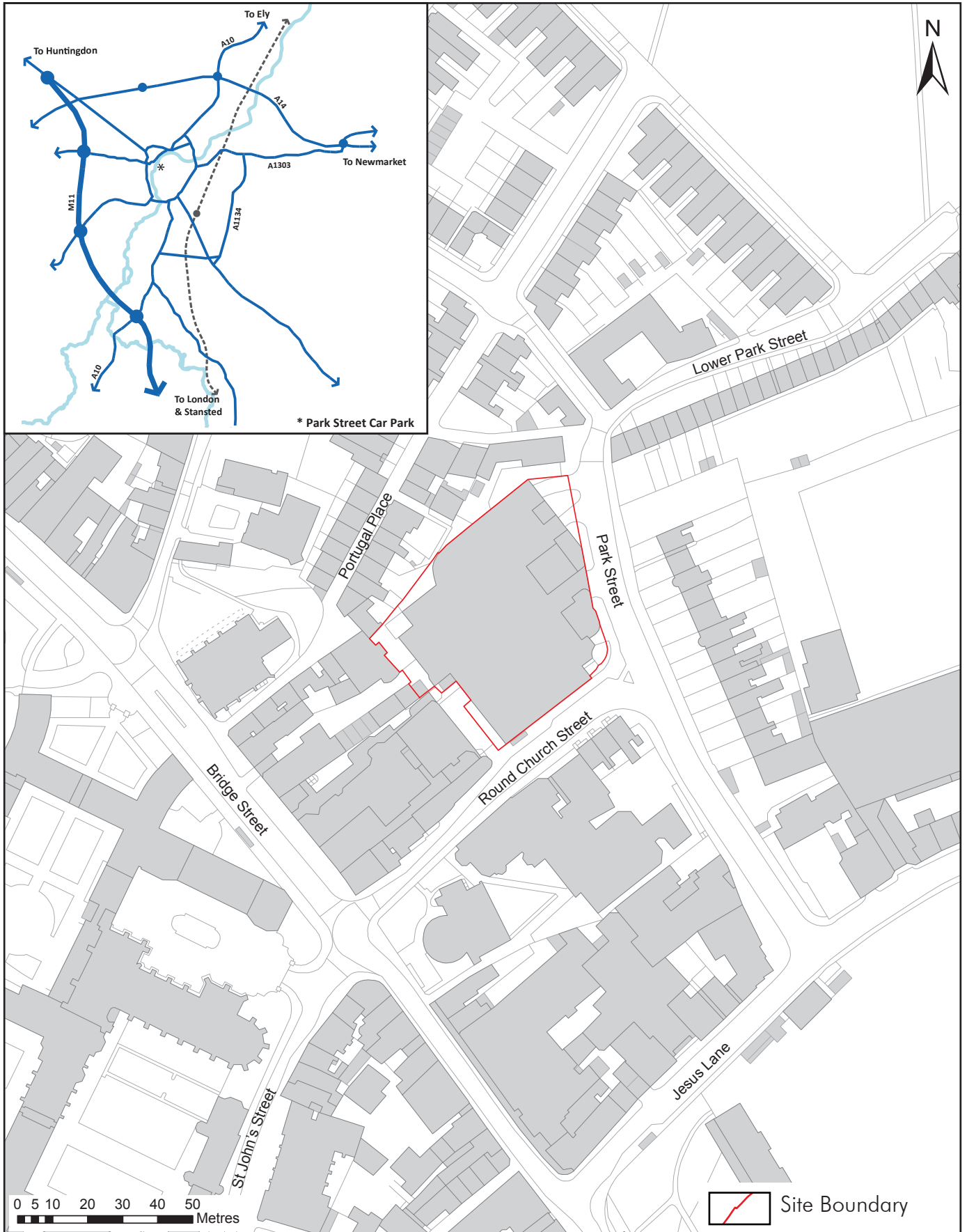


Figure 2: Site location plan

1.3. Site Location, History and Analysis

Site location

1.3.1. Park Street car park is located at the junction of Round Church Street and Park Street within the Central Conservation Area and is identified in Figure 2: Site Location plan.

Site Area

1.3.2. The site is 0.3Ha in size.

Policy designation

1.3.3. The site is not allocated in the Cambridge Local Plan 2006 or the submitted Local Plan (2014) and can therefore be classed as a windfall site.

1.3.4. The National Planning Policy Framework (NPPF) contains a presumption in favour of sustainable development and accordingly the redevelopment should make best use of the land available.

Land ownership

1.3.5. Park Street car park is currently owned by Cambridge City Council. It is intended that the Council retain ownership of the site for the future redevelopment through a development agreement, joint venture or acting as a developer.

Existing land use

1.3.6. The site is currently occupied by a 390 space multi-storey car park, 274 cycle parking spaces, public toilets and a bicycle repair shop.

Cambridge Union Society

1.3.7. At the time of writing proposals for the redevelopment of the Cambridge Union Society site are under discussion.

Site history

1.3.8. The site is located within the Historic Core of Cambridge and as such has been intensively developed and redeveloped over the centuries. By the time the 1886

Ordnance Survey was published, the site can be seen to have a number of small units creating a well-defined edge to Round Church Street and Park Street. The now lost Ram Yard can be clearly seen and it is notable how many buildings are accessed from it.

1.3.9. The 1926-7 Ordnance Survey map shows the site developed as a series of small units grouped around Ram Yard and Jordan's Yard with frontage onto Park Street and Round Church Street. The latter street being much reduced in width when compared with today.

1.3.10. A road widening scheme in 1962 resulted in the narrow row of buildings being removed and the consequential loss of Ram Yard. The 1964 Ordnance Survey shows that the site had been comprehensively redeveloped to create the multi-storey car park devised as part of the Holford Plan (1950) for Cambridge and the re-engineering of the Medieval City to meet the burgeoning needs of the private motor car.

1.3.11. Today the site is still in use as a multi-storey car park. A refurbishment of the car park was carried out in 1997 with cycle parking added in 2002 and refurbishment of the toilets in 2003.



Figure 3: View from St. John's Street towards Round Church Street and Park Street Park Street Car Park

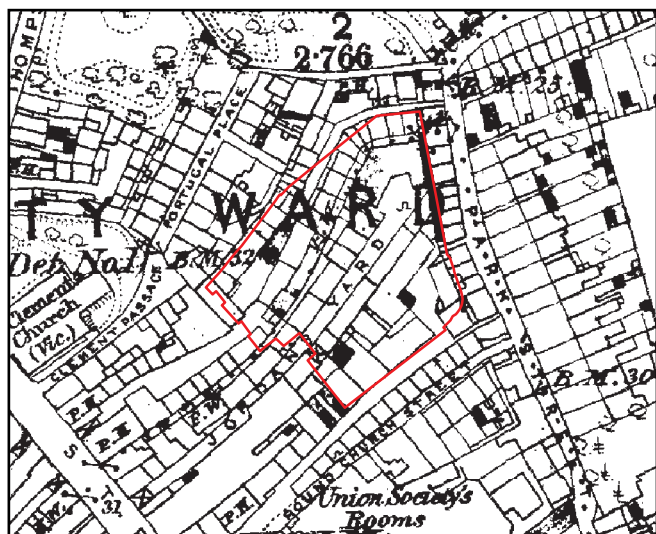


Figure 4: 1886 OS Map



Figure 5: 1926-27 OS Map

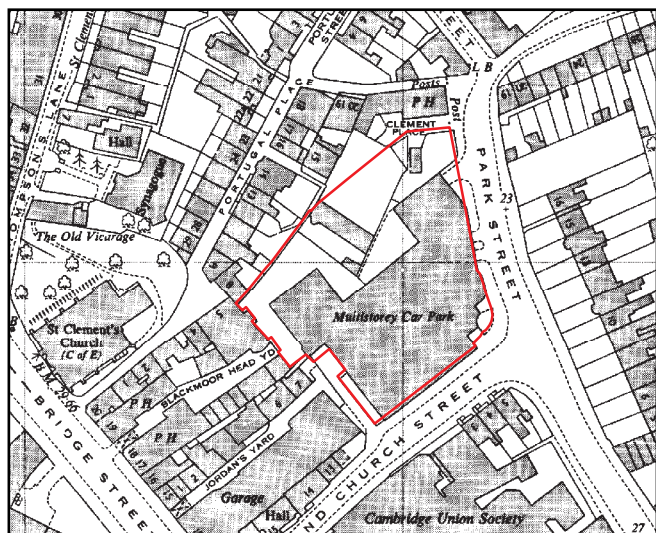


Figure 6: 1964 OS Map

13.12. Jordan's Yard still exists running in a north-east south-west direction and provides access to Park Street Car Park itself along with an entrance into Cambridge Arts & Sciences Sixth Form & Tutorial Collage (CATs). This route was somewhat truncated by the insertion of the car park and wider redevelopment of the area in the 1970s.

Character Appraisal

13.13. To the north west of the site is Portugal Place, a street of high significance in terms of the character and quality of the Central Conservation Area. The 1950s Maypole public house bounds the northern site perimeter. To the south west boundary is CATs. A crane jib is attached to the northern elevation of this building and perhaps hints at the more industrial use of the wider site in the not too distant past.

13.14. The immediate locality shows the historically fine grain of the area with 'yard' type developments typical of the area. A number of these still exist



Round Church Street. This quaint old street with its wood and brick buildings, gay window boxes and cheerful paintwork, runs from Bridge Street to Park Street and it is from the latter entrance that our sketch is taken.

It is set in an area of "change," for long-standing demolitions are now changing the face of the once cobbled Park Street, and it is here, too, that the New Spine Relief Road is scheduled to run its concrete course.

Prziborsky's (established 1879) the old barber's shop, juts out like a lurching tram car at the other end of the street while in the middle of the pedestrian thoroughfare a solitary gas lamp stands sentinel over the central guttering, the best marble alley in all of Cambridge!

St. John's College Tower peeps over the shoulder of the houses, a happy knack it has throughout this part of the city.

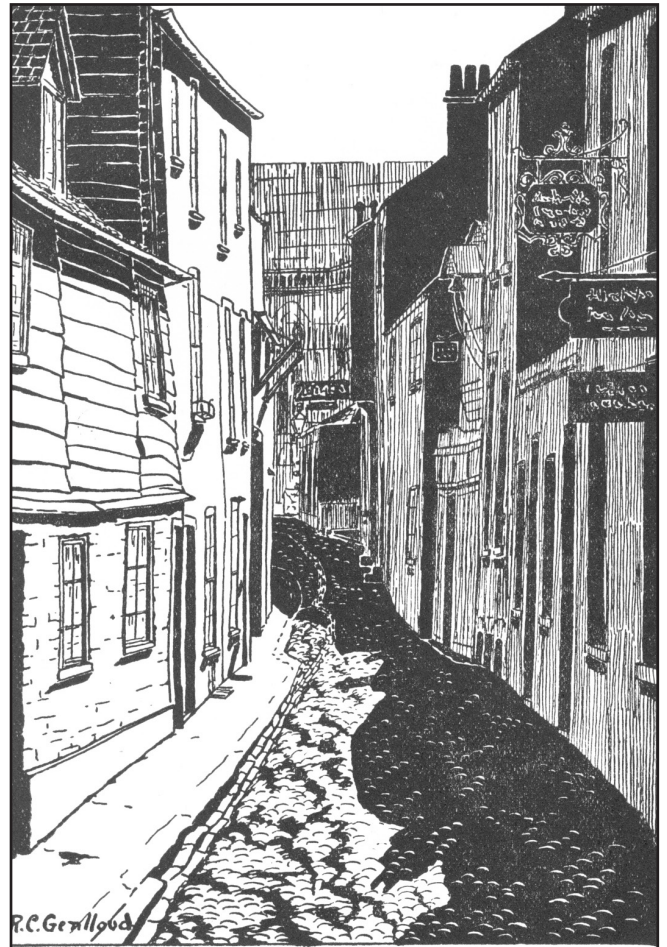
Figure 7: Extract from Cambridge Evening News 1st September 1956 (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)



Figure 8: View looking into Ram Yard, Cambridge Evening News 1960s (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)



Figure 9: View of Ram Yard (left) and Round Church Street (right) from 1935 showing Prziborsky's hairdressing shop in the centre (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)



RAM YARD, BRIDGE STREET

XVII

Figure 10: View looking north from Ram Yard by R.C.Genllood (print),1933 (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)



Figure 11: View looking north from junction of Park Street and Round Church Street towards Lower Park Street 1920s (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)



Figure 12: View from Ram Yard looking west towards St John's College, Cambridge Evening News 1960s (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)

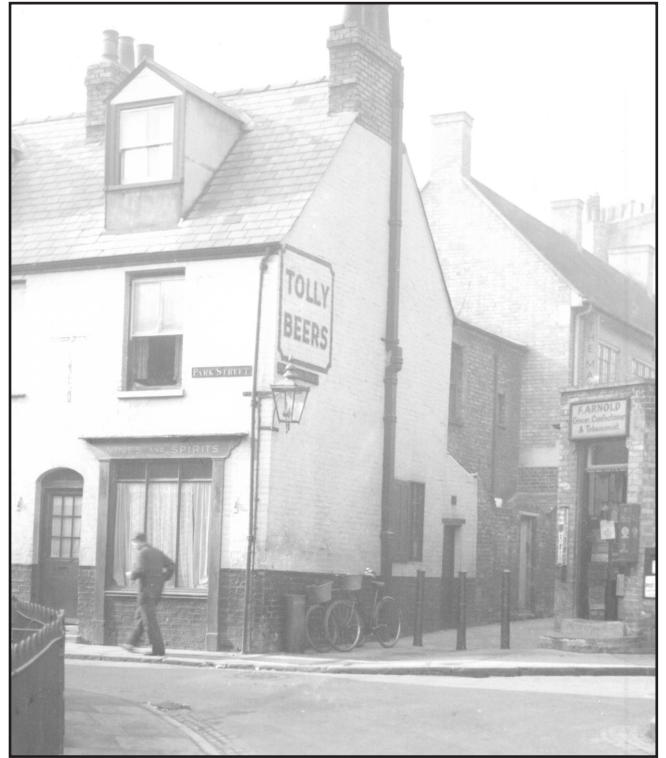


Figure 13: View of the Maypole Pub from corner of Portugal Place and Park Street, 1930s (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)



Figure 14: View of Duck-board house, Ram Yard 1937 (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)

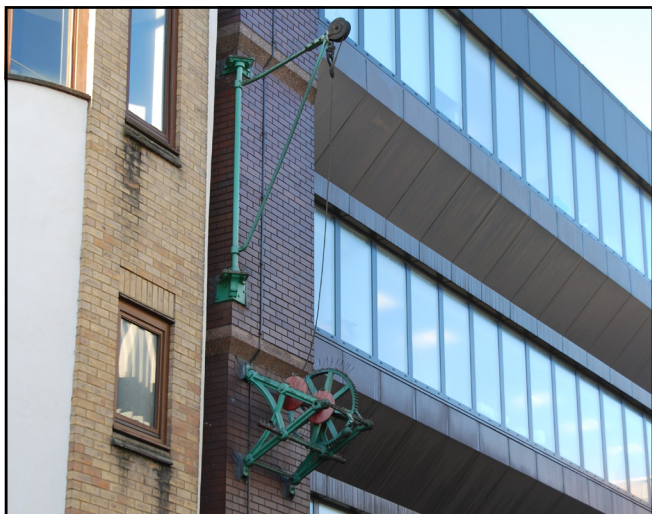


Figure 15: Former warehouse hoist on north-east elevation of CATs



Figure 16: View from Jordan's Yard looking towards Bridge Street



Figure 17: View from Blackmoor Head Yard looking towards Bridge Street

including the truncated Jordan's Yard that provides pedestrian access for the car park. Much of the fine grain development still exists and effectively sleeves the bulkier form of the former County Court building to the south-west of the site.

13.15. The car park itself is clearly a much larger format building and along with the office building (CATs) behind, sit somewhat incongruously in the area. Some attempt was made with car park to break down the overall scale and massing through the introduction of a rhythm of bays and variation in materials.

13.16. The Historic Core Appraisal recognises that the site creates a redevelopment opportunity. As a result, there is the potential to remove the 'back of house character' created by the existing car park.

Designated and Non-designated Heritage assets

13.17. The site sits within the Central Conservation Area that affords statutory protection of the built environment within such an area. Therefore any future redevelopment must demonstrate that it will not harm the character and quality of the Conservation Area.

13.18. There are no listed buildings on the site although buildings on Portugal Place located to the north-west are Grade II Listed and so is the Cambridge Union Society to the south. Accordingly redevelopment proposals will need to demonstrate that they do not harm the setting of these buildings. Buildings of Local Interest at Nos. 5-7, 15 and 19-20 Portugal Place abut the site boundary.

13.19. Figure 18: Opportunities and Constraints, shows the location and proximity of Listed Buildings and Buildings of Local Interest in relation to the Park Street Car Park site.

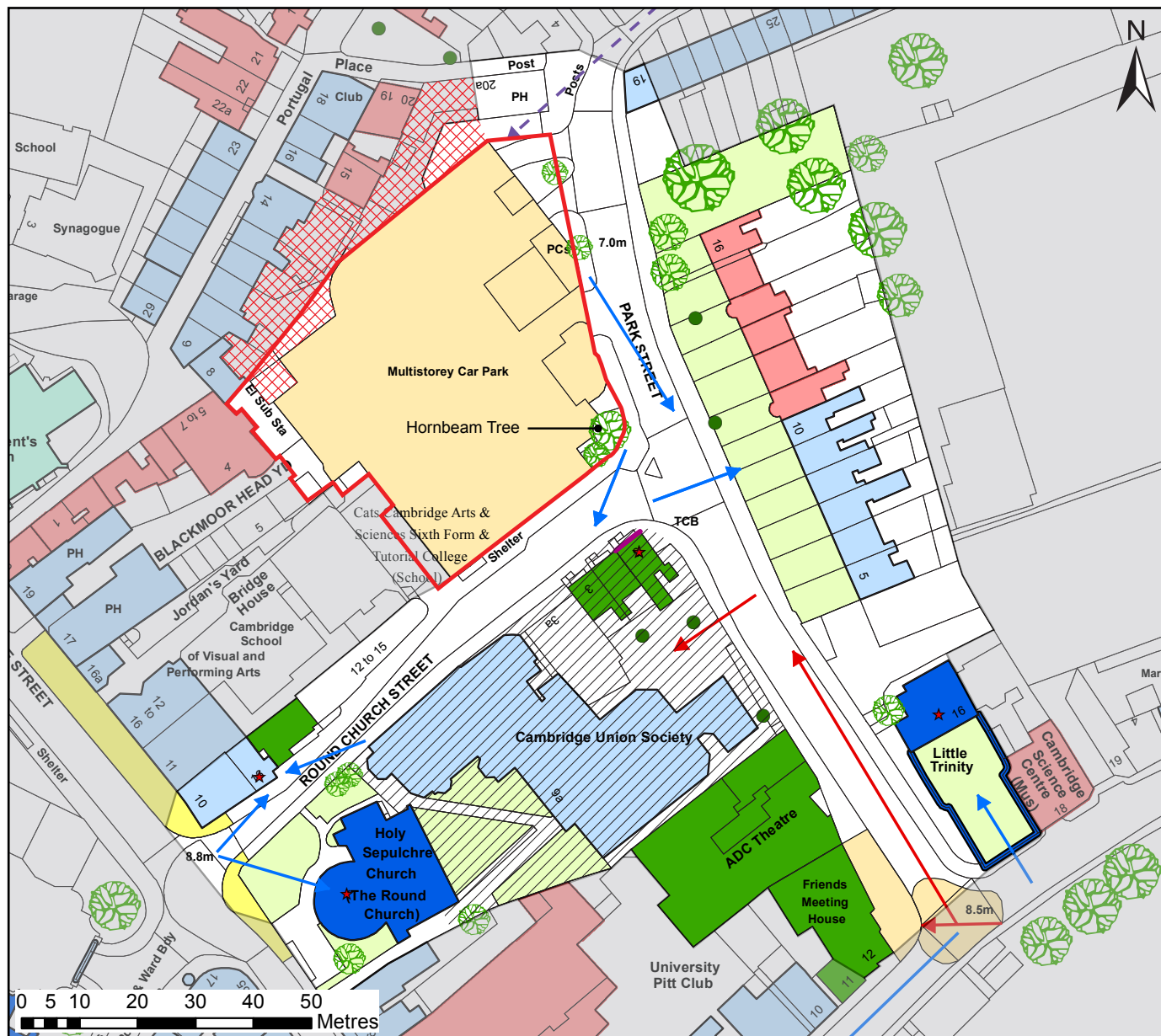





















Figure 18: Opportunities and Constraints in the Park Street Car Park area

- | | | | |
|--|--|---|---|
|  | Site Boundary |  | Key view |
|  | Grade I Listed Building |  | Historic shopfront |
|  | Grade II Listed Building |  | Area of historic pavement |
|  | Grade II* Listed Building |  | Poor quality features |
|  | Building of Local Interest |  | Positive building/structure |
|  | Important Trees |  | Positive green space |
|  | Focal feature |  | Individual Tree Preservation Order |
|  | Poor quality view |  | Sensitive boundary to Portugal Place |
|  | Positive building/structure |  | Possible long view towards St. John's Tower |
|  | Cambridge Union Society potential redevelopment site | | |

Existing scale and massing

1.3.20. When compared to the historic grain of the area, the car park is a large format building that extends to 5 levels and an overall height of approximately 12m across the majority of the building. A taller stair core 'tower' occupies a position at the Round Church Street and Park Street junction and extends to approximately 15m. A second smaller stair core to a similar height occupies a position on the south-west facing section of the car park.

1.3.21. Figure 19 shows the existing scale and massing of the car park and prevailing scale and massing in the immediate area.

Existing movement, circulation and access

1.3.22. Motor vehicle access and egress to the car park is from Park Street to the north. Access to cycle parking in the semi basement cycle park is from Park Street. In terms of pedestrian access, the site fronts onto both Round Church Street and Park Street, with access via stair and lift core on Park Street and from the south-west end of Jordan's Yard.

1.3.23. Future vehicle access locations will need to satisfy Cambridgeshire County Council as Highway Authority.

Constraints and opportunities

1.3.24. Figure 18: Constraints and opportunities summarised the main constraints and opportunities to be considered for the redevelopment of the Park Street car park site.

Tree Preservation Orders

1.3.25. There are no trees on the site covered by Tree Preservation Orders although the Hornbeam tree located to the junction of Park Street and Round Church Street should be assessed for condition and if feasible retained as part of future redevelopment.

Archaeology

1.3.26. A limited archaeological investigation was carried out on the site in 2012. It is expected that a more detailed investigation will be required to establish the presence of archaeological remains. The likelihood of Roman and Medieval finds in this part of Cambridge is high and most usually at a depth of between 2 and 3 metres and given the proximity to the King's Ditch and the location of the site within the historic core, Cambridgeshire Archaeology at Cambridgeshire County Council should be contacted to establish the likelihood of archaeological remains being present on the site if necessary the best course of action for further work and investigation.

Contaminated land

1.3.27. A Geotechnical and Contaminated Land Investigation was undertaken by RSK in 2013. It is expected that further investigations may be required as development proposals progress. Reference should be made to the pamphlet 'Contaminated Land in Cambridge – A Developer's Guide' which provides advice to all persons who are proposing to develop or who are involved in the development of land which may be affected by contamination.

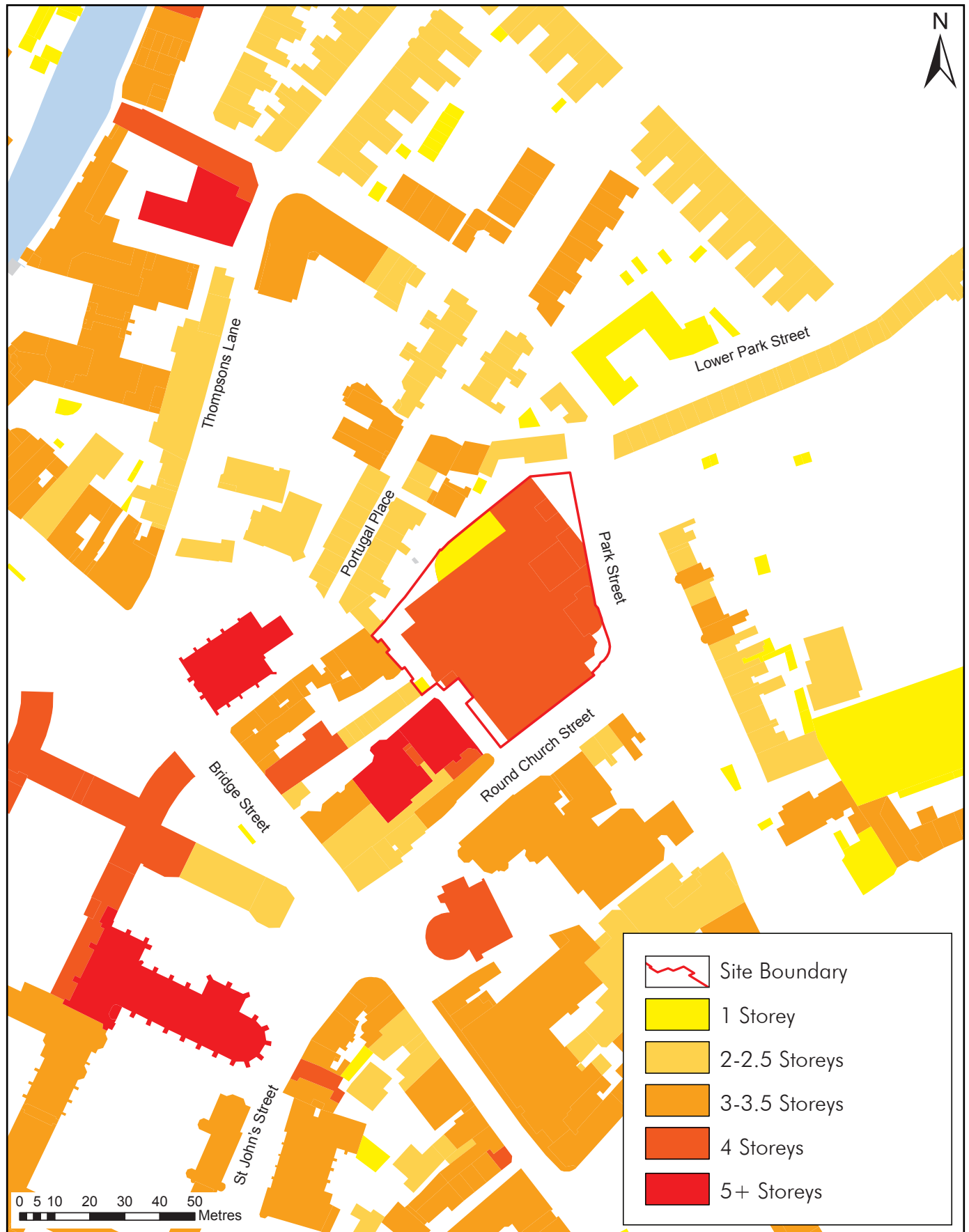


Figure 19: Existing Scale and Massing

2. PLANNING AND DESIGN ADVICE

2.1. Development principles

2.1.1. Key development principles are summarised in Figure 21. These will need to be considered when developing proposals for the Park Street car park site. In addition, further principles are established below.

2.2. Scale and massing

2.2.1. An urban design led approach should be adopted to inform the appropriate scale and massing of redevelopment proposals for the site. This should result in a well-designed scheme that fits into its context, helps to define key entrances and routes and responds to key views and vistas including views into and out of the site into the Conservation Area.

2.2.2. Building heights shown in Figure 21 are expressed as storey heights. It assumes that where commercial ground floor uses are proposed, the floor to ceiling height will typically be around 3.7m (floor to floor height 4m). Residential floors are assumed to have 2.7m floor to ceiling height (3m floor to floor height). Floor to floor heights assumes a 300-400mm construction depth for floors.

2.2.3. There will be some differences in floor to ceiling heights between buildings. These small changes are acceptable and indeed help to create greater variation in roofscape and overall massing.

2.2.4. It is likely that a majority 4 storey development will likely be appropriate on the site, subject to an acceptable design. Building heights should be stepped down to 2 storeys adjacent to the Maypole Public House to create a view to St John's College tower. The rest of the Park Street Frontage should be 3 storeys to better respond to the prevailing scale and massing of the area. There may be

the opportunity for a localised increase in height to the south-west (4+1 storeys) adjacent to the CATs building. The use of setback upper floors will be an effective way of moderating the overall scale and massing of the redeveloped site whilst creating well-articulated forms.

2.2.5. The use of setback upper floors on the two street frontages will allow for a scale of development more suited to the finer grain character of the surrounding area whilst optimising the land available.

2.2.6. Applicants will be expected to produce accurate 3D computer models to inform an appropriate massing of the development on any key views and vistas. Further advice is available in the City Council's Guidance for the Application of Policy 3/13 [Tall Buildings and the Skyline] of the Cambridge Local Plan 2006.



Figure 20: View towards the existing car park from Portugal Place

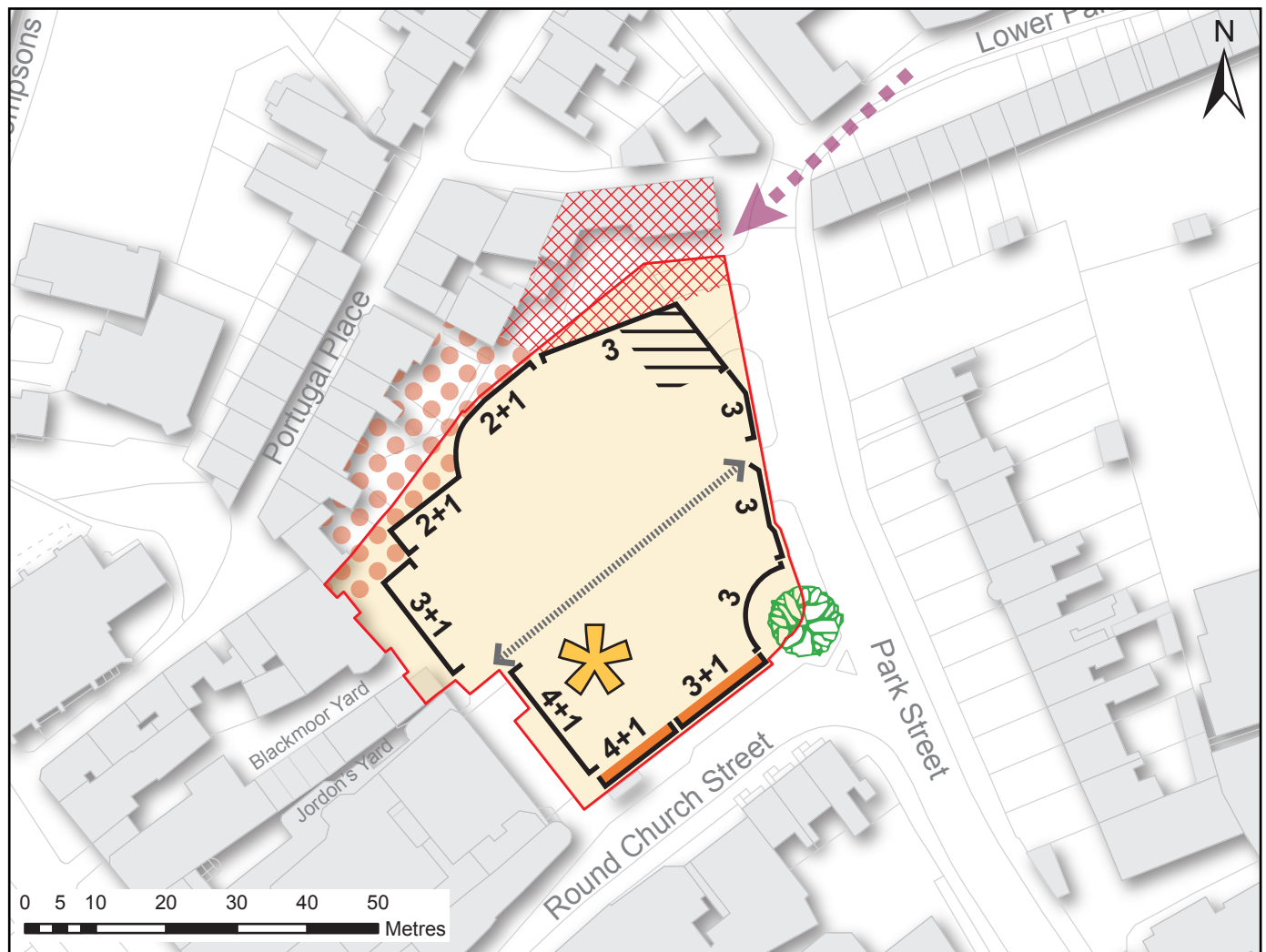












Figure 21: Development Principles

-  Site Boundary
-  Improve setting of pub and beer garden
-  Design to resolve/mitigate potential neighbouring residential amenity issues on Portugal Place
-  Hornbeam tree retained if feasible and subject to condition assessment
-  Possible for increased height adjacent to CATs (4+1 storeys)
-  Possible view to St. John's College Tower over rooftops
-  Possibility of reinstating Jordan's Yard to connect through to Park Street
-  Height limited to 2 storeys to open up views towards St John's College Tower
-  Indicative building frontages
-  Location for possible retail frontage
- 3+1** Storey heights - maximum storey heights indicated assume residential floor to ceiling height of 2.7m (3m floor to floor height). Assume ground floor commercial units would require a floor to ceiling height of 3.7m (4m floor to floor height). Overall height should be inclusive of plant. The +1 indicates accommodation in the roofspace or a setback upper floor (minimum setback 1.5m).

2.3. Key views

2.3.1. The Historic Core Appraisal establishes the significance of the views looking north-east and south-east. In addition the view towards St John's College is a further significant view and the form of development and positioning of built form should respond to this view connecting the finer grain development along Lower Park Street visually through to the greater scale of this important building in the Historic Core of Cambridge.



Figure 22: View towards St. John's College Tower from Park Street



Figure 23: Potential view to St John's College Tower blocked by Park Street Car Park



Figure 24: Views towards St. John's College Tower from Portugal Place

2.4. Finer grain development

2.4.1. The basement car park allows for a more flexible form of development above ground that can respond more appropriately to the finer grain development of surrounding streets.

2.4.2. Proposals should investigate the possibility of reinstating the historic building lines on both Round Church Street and Park Street to work with the finer grain character of the area.

2.5. Rediscovering 'the yards'

2.5.1. Courtyard type schemes could be designed to reflect the historic character of the area. Jordan's Yard should be reconnected through to Park Street. The notion of Ram Yard could be reinstated.

2.6. Retail and commercial uses

2.6.1. If included as part of redevelopment proposals, retail uses could be located on Round Church Street which is the more active of the two frontages. It is envisaged that servicing for retail uses will be from the street.

2.7. Car park access

2.7.1. This will need to be well resolved to reduce the possibility of it negatively impacting on the quality and character of the street. Access is likely to be from Park Street but details will need to be agreed with Cambridgeshire County Council as Highway Authority.



Figure 25: Views over surrounding streets showing finer grain of development

2.8. Public realm

2.8.1. The redevelopment of the Park Street car park should improve the quality of the public realm adjacent to the site boundaries. Where public realm is identified as of positive townscape value in the Core Appraisal, these areas should be sensitively integrated into new development proposals.

2.9. Amenity space

2.9.1. The development should ensure that all residential units have access to private amenity space in the form of roof gardens, balconies and/or winter gardens. It is essential that these amenity spaces are well designed and integral to the character of the development, are located so that they are comfortable to use and are of sufficient size. It is expected that private roof gardens, balconies and winter gardens should:

- Be large enough to accommodate a table and chairs

- Receive direct sunlight for part of the day
- Be positioned away from or designed to mitigate sources of noise and poor air quality

2.9.2. The Hornbeam tree should be retained subject to feasibility and condition assessment.

2.10. Activate the edges

2.10.1. Making frontages ‘active’ adds life, vitality and interest to the public realm. To achieve this aim, development proposals will:

- Create well-designed entrances orientated on to the street to encourage activity within the public realm;
- Maximise the number and size of windows to increase natural surveillance; and
- Use features such as balconies, winter gardens, bay windows to enliven the frontages and articulate façades.

2.11. Cycle provision

2.11.1. Applicants should refer to the Cambridge City Council Cycle Parking Guide for New Residential Developments (February 2010). Cycle parking should be secure, well integrated and convenient to use and make provision for ‘off gauge’ or non-standard bicycles and trailers.

2.11.2. The public cycle parking will be replaced with a facility of the same or greater size.

2.12. Sustainable development guidelines

2.12.1. Creating a sustainable development should be a priority underpinning the redevelopment of the Park Street car park site. A site wide approach

should be employed to address environmental, social and economic issues. Development should comply with the Cambridge Sustainable Design and Construction SPD and propose strategies for the following relevant issues:

- Health and well-being of future residents;
- Energy efficiency of new buildings;
- Design for climate change adaptation;
- Water use;
- Flood mitigation including SUDs;
- Use of materials and resources;
- Waste and recycling;
- Employment opportunities;
- Pollution; and
- Transport and mobility.

Ecology

2.12.2. There will be many opportunities for enhancing the ecology and biodiversity of the site. Initiatives that could be considered are:

- Tree and other planting where appropriate
- Water resources in association with SUDs and other landscape features
- Nesting opportunities for a variety of bird and bat species
- Habitats for insects
- Brown or green roofs

Drainage and flood risk

2.12.3. Part of the Park Street Car Park site is located within Flood Zone 2 on the

Environment Agency flood maps and the site is known to have a high groundwater levels. The lower section of the car park, currently used for cycle parking, has flooded in recent years. The historic alignment of the King's Ditch is in close proximity to the site.

2.12.4. When the site is redeveloped, it will be expected that attenuation will be required on site and given the proximity to the River Cam, such measures will be required to safeguard or improve the water quality leaving the site.

Recycling and waste facilities

2.12.5. It is expected that any development which comes forward on the site will successfully integrate refuse and recycling facilities and clearly separate commercial and residential waste streams.

2.13. Public art

2.13.1. Public art is encouraged as part of development proposals in accordance with Policy 3/7 of the Cambridge Local Plan (2006). The engagement of an artist should be undertaken at an early stage of the design process to ensure that it is well integrated into proposals.

2.14. Planning obligations

2.14.1. The development is likely to result in increased demands for community infrastructure such as public open space, sports, health and community facilities. Some of these demands may be met on site but others will be secured through commuted sums to provide new or enhanced infrastructure off site. Planning Obligations via a Section 106 agreement will be needed to deliver this infrastructure. The full list and scope of these Planning Obligations will be defined through the consideration of the planning application(s) for the site.

Glossary

Active frontages

An active frontage is one which allows some kind of movement or visual relationship between the person outside and the activity inside. At a minimal level, this interaction might be one of simple observation such as a window display or people working. At a higher level of interaction, the pedestrian could be encouraged to enter the unit to buy something or participate in an activity. The most interactive frontages are usually those of cafés, bars or shops, which spill out onto the street.

Articulation

The expression of the vertical or horizontal subdivision of a building facade into perceivable elements by the treatment of its architectural features.

Biodiversity

Encompasses all aspects of biological diversity, especially including species richness, ecosystem complexity and genetic variation.

Building line

The line formed by the frontages of buildings along a street.

Built form

Buildings and their structures

Buildings of Local Interest

Buildings of Local Interest are not subject to statutory protection, but are recognised as being of importance to the locality or the city's historical and architectural development.

Cambridge Local Plan 2006

The Cambridge Local Plan 2006 sets out policies and proposals for future development and land use to 2016; the Plan will be a material consideration when determining planning applications.

Cambridge Local Plan 2014

The Cambridge Local Plan 2014 sets

out policies and proposals for the future development and land use to 2031; the plan will be a material consideration when determining planning applications.

City Centre

Historic Core and Fitzroy/Burleigh Street shopping areas in Cambridge. These areas provide a range of facilities and services, which fulfil a function as a focus for both the community and for public transport. See also Cambridge Proposals Map (October 2009).

Conservation Area

Areas identified, which have special architectural or historic interest, worthy of protection and enhancement.

Fine grain

The quality of an area's layout of building blocks and plots having small and frequent subdivisions.

Form

The layout (structure and urban grain), density, scale (height and massing) and appearance (materials and details).

Historic Core Conservation Area Appraisal

The Historic Core Conservation Area Appraisal covers 70+ streets in the city centre which are defined according to their significance. This significance can be their historical, architectural or social impact on the character and appearance of Cambridge.

Listed Building

A building or structure of special architectural or historic interest and included in a list, approved by the Secretary of State. The owner must get Listed Building Consent to carry out alterations that would affect its character or its setting.

Massing

The combined effect of the arrangement, volume and shape of a building or group of elements.

Mixed use development

Development comprising two or more uses as part of the same scheme. This could apply at a variety of scales from individual buildings, to a street, to a new neighbourhood or urban extension. 'Horizontal' mixed uses are side by side, usually in different buildings. Vertical mixed uses are on different floors of the same building.

Movement

People and vehicles going to and passing through buildings, places and spaces.

Natural surveillance

The discouragement to wrong-doing by the presence of passers by or the ability of people to see out of windows. Also known as passive surveillance.

National Planning Policy Framework (NPPF)

The NPPF sets out the Government's planning policies for England and how these are expected to be applied. It sets out the Government's requirements for the planning system only to the extent that it is relevant, proportionate and necessary to do so. It provides a framework within which local people and their accountable councils can produce their own distinctive local and neighborhood plans, which reflect the needs and priorities of their communities.

Permeability

Permeability describes the degree to which urban forms, buildings, places and spaces permit or restrict the movement of people or vehicles in different directions. Permeability is generally considered a positive attribute of urban design, as it permits ease of movement by different transport methods and avoids severing neighbourhoods. Areas which lack permeability, e.g. those severed by arterial roads or the layout of streets in cul-de-sac form, are considered to discourage effective movement on foot and encourage longer journeys by car.

Planning Obligation

A binding legal agreement requiring a developer or landowner to provide or contribute towards

facilities, infrastructure or other measures, in order for planning permission to be granted. Planning Obligations are normally secured under Section 106 of the Town & Country Planning Act 1990 (as amended).

Public Art

Publicly sited works of art, which make an important contribution to the character and visual quality of the area and are accessible to the public.

Public Realm

The parts of a village, town or city (whether publicly or privately owned) that are available, without charge for everyone to use or see, including streets, squares and parks.

Section 106

See Planning Obligation.

Sustainable Development

Sustainable Development is a very broad term that encompasses many different aspects and issues from the global to local levels. Overall sustainable development can be described as 'Development, which meets the needs of the present without compromising the ability for the future generations to meet their own needs' (after the 1987 Report of the World Commission on Environment and Development – the Brundtland Commission).

Sustainable Drainage Strategy (SuDS)

Development normally reduces the amount of water that can infiltrate into the ground and increases surface water run-off due to the amount of hard surfacing used. Sustainable drainage systems control surface water run off by mimicking natural drainage process through the use of surface water storage areas, flow limiting devices and the use of infiltration areas or soakaways etc.

Urban Grain

The pattern of the arrangement and size of buildings and their plots in a settlement; and the degree to which an area's pattern of street blocks and street junctions is respectively small and frequent, or large and infrequent.

List of Figures

Photographs and images were taken or created by Cambridge City Council unless otherwise stated.

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Figure 2: Site Location plan

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Figure 12: View from Ram Yard looking west towards St John's College, Cambridge Evening News 1960s (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)

Figure 13: View of the Maypole Pub from corner of Portugal Place and Park Street, 1930s (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)

Figure 14: View of Duck-board house, Ram Yard 1937 (Image courtesy of Cambridgeshire Collection, Cambridge Central Library)

Figure 15: Former warehouse hoist on north-east elevation of CATs

Figure 16: View from Jordan's Yard looking towards Bridge Street

Figure 17: View from Blackmoor Head Yard looking towards Bridge Street

Figure 18: Opportunities and Constraints in the Park Street Car Park area

Figure 19: Existing Scale and Massing

Figure 20: View towards the existing car park from Portugal Place

Figure 21: Development Principles

Figure 22: View towards St. John's College Tower from Park Street

Figure 23: Potential view to St John's College Tower blocked by Park Street Car Park

Figure 24: Views towards St. John's College Tower from Portugal Place

Figure 25: Views over surrounding streets showing finer grain of development

